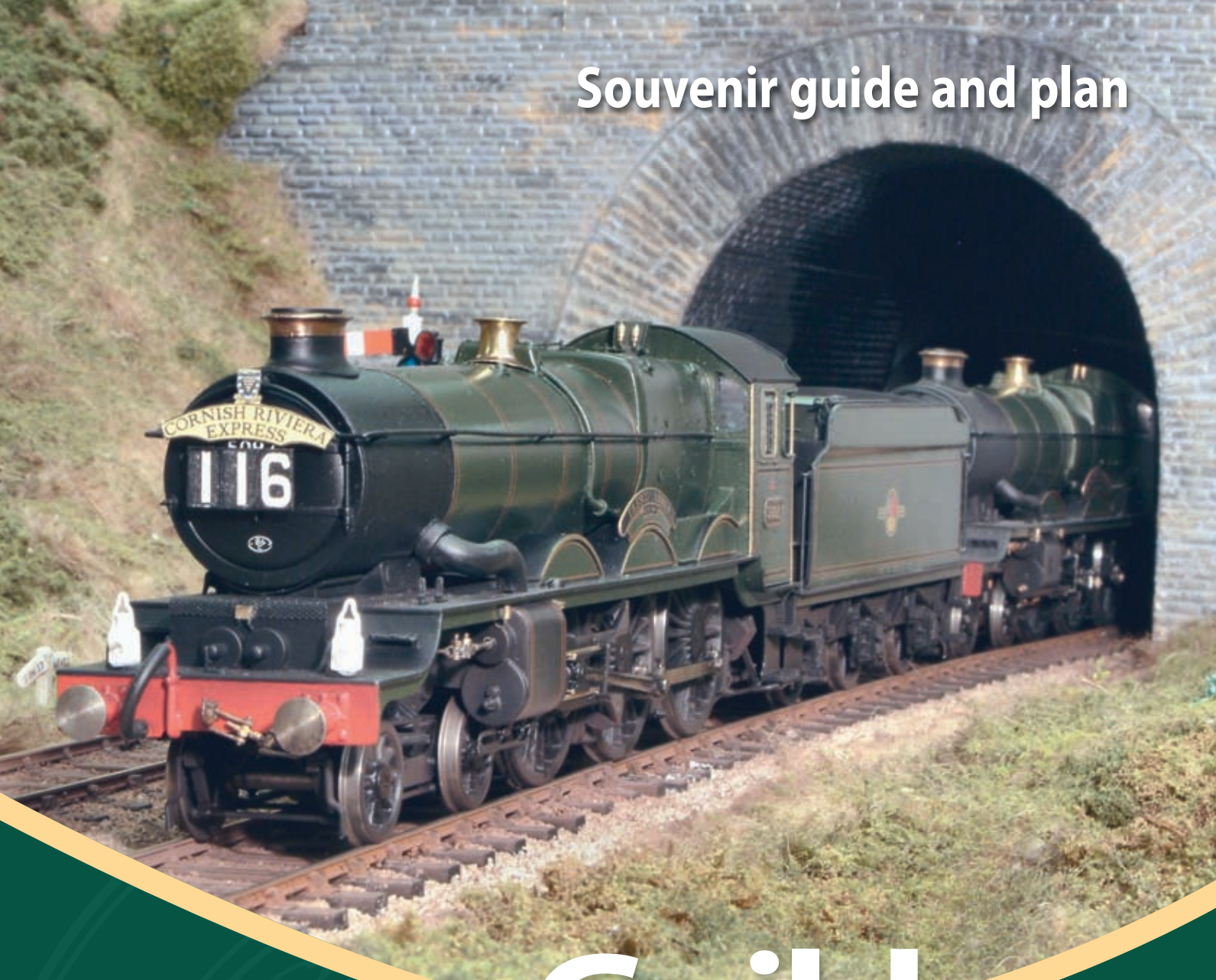


Souvenir guide and plan



# Guildex 2012

Telford International Exhibition Centre  
St. Quentin Gate, TELFORD, Shropshire TF3 4JH

Saturday 8th September 10.00am - 5.00pm  
Sunday 9th September 10.00am - 4.00pm

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# Guildex 2012

L4 Schwanchhausen

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John Rich

**Cover:** Dainton Bank  
Photo courtesy Tony Wright BRM

## Welcome to Guildex 2012

Once again we are in halls two and three. There is a different entrance this year, so please follow the signs and if in doubt ask the stewards to direct you to it. When you arrive at the entrance please listen for announcements and make yourself known to the door team if you have an advance ticket.

Following feedback from last year's show we have amended some of the demonstrations in line with your suggestions. Many of them will now take the format of hands on tuition to give you a feel for how a task is done. Two of our most experienced demonstrators (Tony Reynolds and Malcolm Mitchell) have asked for a rest this year, but will still be attending as visitors. You will find further information about the demonstrations within the Guild News received with this guide.

We are not running the mini lectures this year due to the changes in how we demonstrate, but these may be run again in future.

As always this guide is the most up to date information we have when going to press. If there are significant changes these will be posted on the website prior to the show and a large map will be displayed by the entrance during the event. If you are unsure where something is please ask at the Guild information stand.

If anyone needs first aid when attending the event, can you please make yourself known to a Guild official who will direct you to the right place. First aid is available on site provided by the exhibition centre.

Please remember that if you bring anything of value to the show that you have adequate insurance for your belongings. If you have entered something into the competition and have provided details in advance to the competition organisers then these are covered.

We have once again held the entrance fee for members at five pounds. Please encourage fellow members to attend so that we can make this another successful Guildex. Enjoy the show and if you get the opportunity talk to one of the Events Team to give your feedback.

**Nigel Smith**  
Chairman of Events

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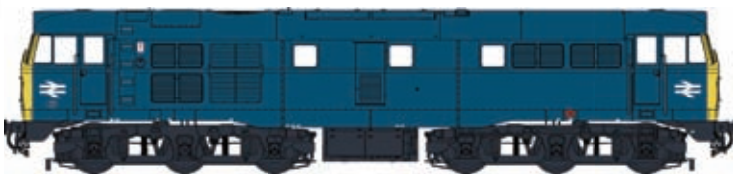


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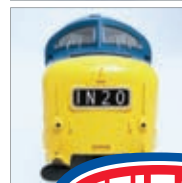
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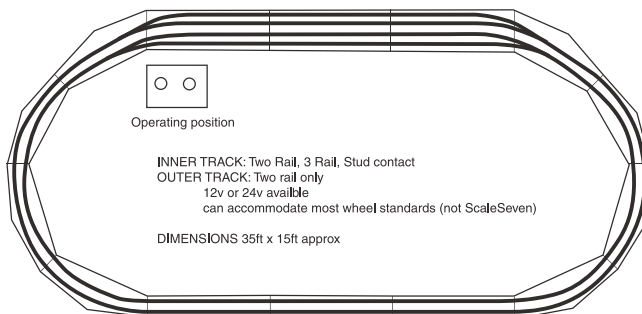
L2 Western Avenue

## L1 East Midlands Test Track

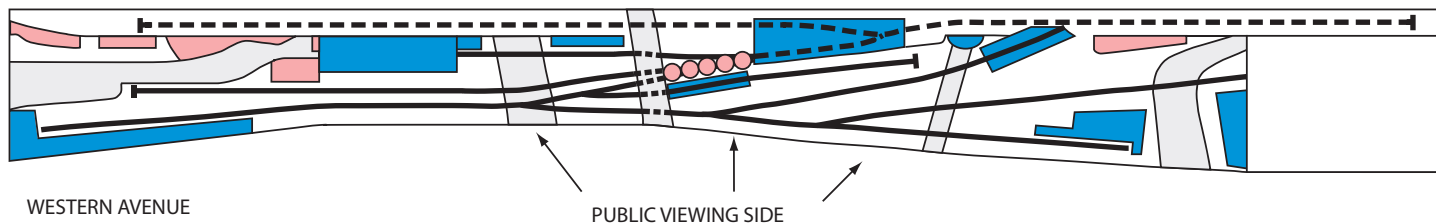
**Nelson Keen**

**Dimensions: 10.7m x 4.6m**

The East Midlands 0 Gauge Group's test track can accommodate most wheel standards, with the exception of Scale 7 and some early, extremely coarse examples – there's not much that has defeated it – to 12v or 24v DC transmitted via 2-rail, centre 3rd-rail or stud contact (using the third rail as pick up) or live steam. Clearances between running lines permits running of 16mm/ft scale models running on 32mm track gauge.



L13 Test Track



L2 Western Avenue

## L2 Western Avenue

**JH Dodd**

**7m x 2m**

Western Avenue is in the industrial downtown area of the City of Lyttelton. It is north west of Chicago, served by numerous class 1 railroad companies. The local township elders decided to form their own railway company, to move traffic from the transfer yard off the main line, to the city. The Lyttelton and Western Railway Company was born.

The L&W own several cast off and secondhand forms of motive power, in various states of readiness, so from time to time hired help is required to serve the local businesses.

Trains arrive at the city yard, situated just beyond the Western Avenue flyover, and are dispatched to the local companies. Here they are switched to the correct track for loading or unloading as required.

The buildings are a mixture of scratch or modified card kits, made to fit the situation. No freight cars are allowed on the layout without a destination so you will only see cars with a reason to be there. This is a DCC controlled layout, but with conductor (hand) operated points and participation is often encouraged. Please feel free to ask any questions, we will try to give a sensible answer.



L3 Racoon Canyon

## L3 Racoon Canyon

**Tony Kell**

**Dimensions: 6m x 1.3m**

Some time ago I got the On30 bug. I'm still not cured and so as a form of therapy I built this layout which is based loosely on American prototype set high in the American mountains where The Arithmetic Either Ore is mined. This is sent to 'Algebra' down in the valley along the narrow gauge line where it is worked out. In addition to this, no...no more mathematical puns! Enough!

Unfortunately the bears seem to have eaten all the racoons so I don't see any left in the hills around the mine but there are a number of other critters scuttling about the place.

The layout is operated with a Lenz DCC system and many of the locos have 'on board'

# Layouts

sound systems....chuff chuff chuff, clang clang...You get the picture.

The track is PECO 7mm narrow gauge track painted and weathered to make it look old. The mountains are made of polyurethane foam boards cut with a kitchen carving knife and coated with ARTEX prior to painting with emulsion paint I had specially mixed at a DIY store.

Some of the buildings are American laser cut wooden kits and some are scratch built.

The layout breaks into three large front boards and three fiddle yard boards for transportation. A point control panel fits to the rear and all the points are linked to the panel with computer style cables. The points are operated with SEEP point motors.

The DCC system is all linked together with Phono type cables. With the power unit on the floor under the layout. The lights have low wattage energy saver bulbs on the pelmet.

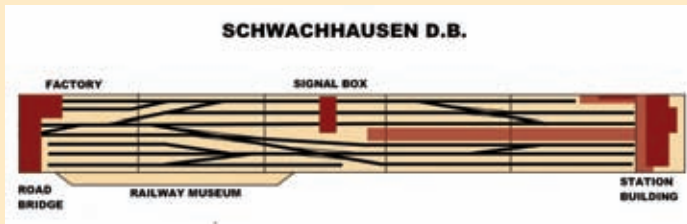
If you have any questions, please go ahead and ask. Whether you will get a sensible reply...well ask and you find out. I hope you like the layout.

## L4 Schwachhausen

**Peter Smith**

**Dimensions: 12.2m x 1m**

Schwachhausen is a real place, a suburb of Bremen in northern Germany, but it never had a railway. My layout represents an urban terminus that might have existed serving the north of the city, with through trains from Hamburg and a lot of local services. The period is the Summer of 1962 when steam was still very much in charge but new diesel classes were beginning to be seen. The two platform station has a large station building across the end, modelled on nearby Wildeshausen, and the double track passes under a gantry signal box before running under a road bridge and out of sight. There is a small goods yard with a private siding serving an engineering works, and a small loco depot for servicing of



the loco's between trains which gives the excuse for plenty of light engine movements.

Most of the passenger trains are the routine local services, but occasionally a special train of museum bogie coaches uses the station hauled by a preserved Pacific loco in pre 1924 livery. Freight traffic is the usual vans and open wagons, plus a block train of tank wagons that comes into the station from a local depot to run round before heading off up the main line. A PW train can also be seen which is based at the station.

The layout uses Lenz DCC for control and all the loco's have sound. The track is also Lenz as is most of the rolling stock, but the loco's also come from Kiss, Gebauer, MTH & Bundgen and there are wagons from Brawa. Lenz couplings are fitted to all the stock as the Lenz loco's feature automatic uncoupling which makes shunting a real pleasure and which wouldn't be possible with scale couplings. Everything has been weathered, and three wagons have scratchbuilt bodies on a Lenz chassis. The points and signals are controlled from the handheld unit using Hoffmann point motors, so there is no control panel and there are no switches. ....the hand held controller does everything.

The buildings are all scratchbuilt, and the backscene uses photographs from the real Schwachhausen to give at least a flavour of the real place as well as others showing industrial buildings from northern Germany. Schwachhausen is quite a leafy suburb, but had the railway been built it would no doubt have attracted a lot more industry. Happily, all the original buildings seem to have survived the bombing of Bremen during the war! The figures are from Preiser.

We welcome questions from visitors and are always happy to explain things and chat about the layout, so don't be afraid to approach one of the operators. More pictures and details of the layout can be found at [www.smithslayouts.com](http://www.smithslayouts.com)

## L5 Wrexham Hightown

**By the Christleton (must have some beer) Group**

**Dimensions: 4.8m x 1m**

The model depicts a three road LNWR locomotive shed as it may have looked in the early 1950s, and we have chosen the Hightown area of Wrexham as our location. This is based on some historical fact that the LNWR promoted a number of schemes to bring a branch

L3 Racoon Canyon



L4 Schwachhausen



L4 Schwachhausen





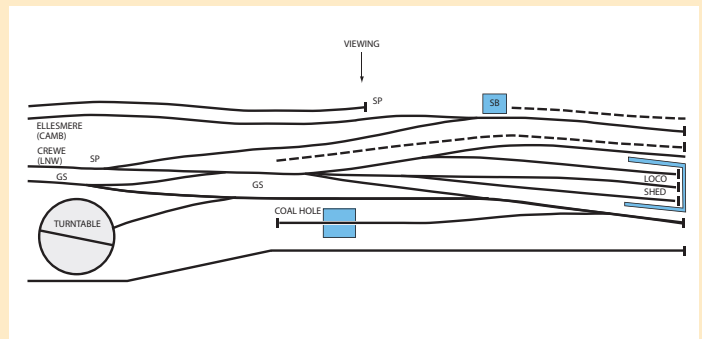
L5 Wrexham Hightown



L5 Wrexham Hightown



L6 Sykes Bridge Shed



into Wrexham from the east and supported by local business men in the hope of breaking the GW's monopoly. With its coal mining, steel, brick and tile making the traffic potentials were high added to this brewing, sand extraction as well as agricultural produce it is surprising a line was never built!

Using this as a basis, with some artistic licence to allow the inclusion of the Cambrian Ellesmere line, we try to portray the atmosphere of a typical branch shed of the period by using the type of stock associated with the area. To add interest we have included a through running connection between the two companies which was added as a wartime necessity thus allowing the close of the LMS station at Yorke Street. As this station never reopened the LM passenger trains continue to use the Cambrian's facilities at Wrexham Central. A sequence of prototypical movements has been worked out based on working timetables of the period, covering the area modelled, our premise being authenticity. Site visits were made to Wrexham and parts of Ellesmere branch that can still be traced to help get a feel of the topology and the architecture used. Other information on various aspects of the layout comes from careful research.

The viewing area is some fifteen foot by three with road bridges at either end to form view blocks, the back being made up of a sleeper built fence for the same reason, all to contain the eye. Baseboards are constructed using plywood throughout from box section for the main supports, with open framework above for supporting scenery and track bed. This method provides for both rigidity and a degree of lightweight needed in a portable layout. A covering of papier mache and chicken wire is used to build up the land in between the track and this is then covered with carpet underlay to form a base on which scenery can be developed.

Buildings are all based on prototypes and are mostly scratchbuilt using ply and plasticard with two from commercially available kits. Track work is made using C&L components. The signals are home made using commercially available parts as necessary and other detailing items that are seen are either made or brought in. Our objective has always been to create an authentic scene in miniature and again it pays to carefully research each subject so that it all gels together. The layout is far from complete as we still need to add detail. We hope that even in its current form you enjoy this historical model as much as we have building and researching it. For your interest members are;

Arthur Mcgee: promoter/chief engineer

David Burns: chief assistant/recorder

Andy Littler: under baseboard manager/secretary

Steve Cockery: can lad (beer)

Ably assisted from time to time by members of the Chester Model Railway Club

## L6 Sykes Bridge Shed

**Merrick Thompson**

**Dimensions: 7.4m x 1m**

In 1838, the North Union Railway built the line from Wigan to Preston and constructed the first engine shed sited to the southeast of Preston station at Butler Street. In 1848 the Lancaster & Preston Railway also erected a shed at Preston located to the north of the station.

By the turn of the century, the north shed has been extended with a total of 15 roads and became the main shed for Preston. Sykes Bridge was relegated to sub shed status, consisting of a four-road shed with four outside storage roads, manual coaling facilities and a 50' turntable.

In 1936, the LMS drastically altered both depots in line with their 'Modernisation and Efficiency Plan'; the central idea being that locomotives should follow a logical sequence when coming onto shed for servicing. Ideally, the disposal operation would be as follows:

- coaling with simultaneous watering (if possible)
- ash disposal/fire cleaning
- Turning

# Layouts

d) Stabling in shed for examination or return to further duties

Sykes Bridge was relaid to this 'ideal plan', with adaptations dictated by the restrictions of the site, with installation of mechanised coaling and ash plants, the resiting of the turntable but with a new 70ft unit and increasing the shed size to six roads.

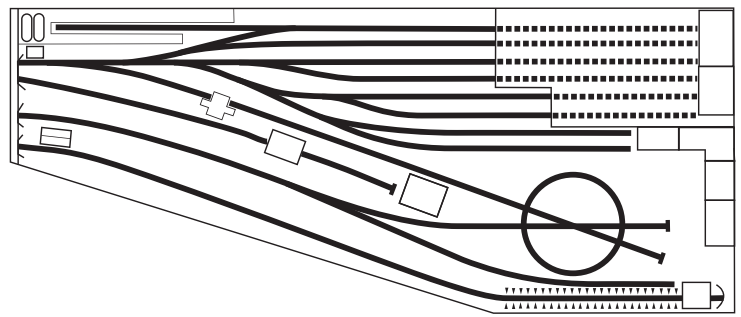
Sykes Bridge shed remained as a sub shed of Preston with a variety of passenger and freight locomotives. An ex L&Y 'Pug' saddle tank was allocated as shed pilot and used for working the Greenbank Yard with its tight curves, Jinties and Ivatt class 2s for working the North Union and Dock Street Yards as well as station pilots with ex LNWR Super 'D's and ex MR 2Fs for the Ribble Dock and general freight duties. Freight duties included workings to Kirkham Sidings, St Annes Yard, the Pilling to Preston Goods (known locally as the 'Pilling Pig'), coal and banana trains from Preston Docks to Bamber Bridge Yard and for the Longridge branch to Deepdale Yard.

Black 5s, Jubilees and BR Standard class 4s and 5s were allocated for express turns to Barrow, Windermere, Liverpool, Manchester, Blackpool and Southport with an allocation of one or two Royal Scots for use on the London or Carlisle workings.

In the summer months, Sykes Bridge would be used as an overflow facility for Preston Shed and could be host for up to 25 locomotives including Coronation and BR Standard Pacifics from other depots such as Crewe and Carlisle.

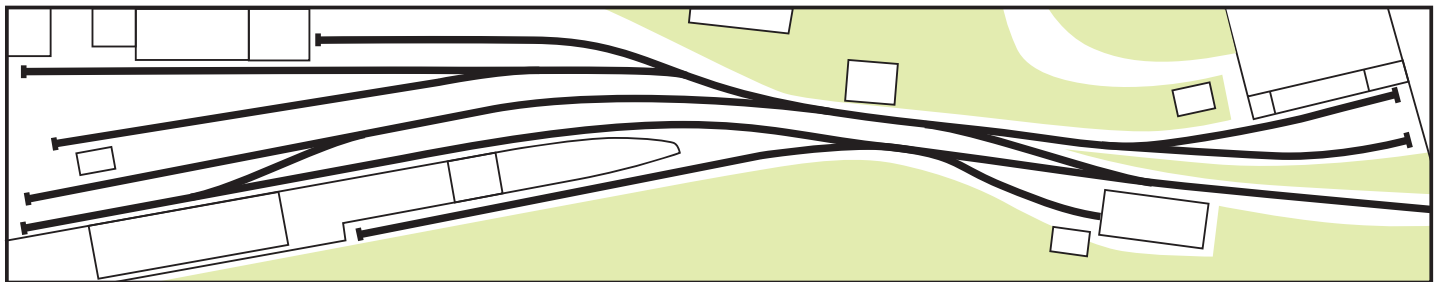
With the arrival of the modernisation to diesel traction, an area of the MPD has seen the provision of a small fuelling depot and a road of the shed allocated for diesel maintenance.

The layout is an attempt to portray Sykes Bridge Shed as it would have appeared in the mid 1950s with representatives of all actual locomotives that were to be seen in and around Preston during the period.



## L7 Harvistoun

**Stirling & Clackmannan District Model Railway Club**  
**Dimensions: 11m x 1m**



Harvistoun was created and is maintained by a small band of O gauge enthusiasts within Stirling and Clackmannan District Model Railway Club.

The model represents a typical branch line terminus located in the Hillfoots area of Clackmannanshire which lies to the south of the Ochil Hills near Stirling.

The passenger and goods facilities as provided on the model are representative of those which existed at locations such as Alva, Menstrie or Dollar, the latter on the former Devon Valley line between Alloa and Kinross.

The chosen period is the 'transition era' covering the 1950s and 60s when steam motive power was still widespread but the internal combustion engines were beginning to make their presence felt. The layout depicts a part of Scotland's railway network when rail still served such rural parts of the country.

You are invited to linger a while by the line side and enjoy the trains running by. Whether to simply watch or to photograph, we trust you derive as much pleasure from the presentation as our members. Do not hesitate to ask questions of the operators. Further information regarding Stirling and Clackmannan District Model Railway Club may be obtained from the Club website which can be found at: [www.stirlingmrc.co.uk](http://www.stirlingmrc.co.uk).



## L8 Mines de Villers-Nordicourt

**Nick Page**  
**Dimensions: 3.7m x 1m**

Coal made the Industrial Revolution.

The Départements du NORD et du Pas de Calais (the northern departments of France) are well known for mining. There were coal mines in different places all around the country, but let's fix it there...





L8 Mines de Villers-Nordicourt



L8 Mines de Villers-Nordicourt

## WHY?

When we decided to build the layout in 2006, Frederic and I had to make up our minds on the project. We both were into industrial railways and both enjoyed pre-war rolling stock. We also wanted the layout 'somewhere in Picardy,' after our first attempt with Dunkirk Marine (exhibited at Guildex in 2002 and 2006). So we decided to create Villers-Nordicourt.

## HISTORY

Villers-Nordicourt is in Picardy... the north of Picardy really, not far from the border with Pas de Calais. The place is situated about 10 miles north west of Peronne, in the very heart of the Battles of the Somme and it had the only coal mine in Picardy.

The Chemin de Fer du Nord company was quick to start carrying coal from the northern mines to feed the industries in the Paris area. In the 1870s, lines were built around Cambrai by the 'Flandres & Picardy' Company to challenge the rich Compagnie du Nord. It was absorbed by its larger rival in 1885 but later, by the turn of the century, the Compagnie des mines d'Anzin, built a line to join the town of Villers-Nordicourt to the northern coal fields.

As said before, from 1914 to 1918, the mines were in the very front line of the battlefields. At the Armistice, on November 11th 1918, all was devastated and the poor quality of the coal, as well as the small quantity still available there, did not make it worth renovating the mine.

From there it all disappeared from popular memory and has since been forgotten.

## RIGHT or WRONG?

You've just read the very exclusive history of les Mines de Villers Nordicourt - as we have invented it as the background of the layout.

It is well known that there was no coal anywhere in Picardy so we invented that as well for our 'local' layout!

We have chosen to settle our layout around 1910, just before it disappeared. Of course we allowed ourselves some details out of date, just because we enjoyed it (the lorry is post-war!). We decided to have two operating companies (Nord and Anzin), in order to increase the variety of the rolling stock, and to make it more 'exotic' too!

We also wanted to use our existing NORD rolling stock and try not to build too many new wagons (all our rolling stock is handmade) but there are some exceptions! After our experience on Dunkirk Marine, we wanted to have a layout where we could shunt a little bit more than we did previously! The track plan was discussed for hours before we found a scenario we liked.

We built all buildings following old postcards, in order to create an environment that would be realistic. Bricks are not to everyone's taste but they are the popular choice for buildings in Picardy and Nord-Pas de Calais. Last but not least, the layout had to be carried in an estate car, in order to keep the expenses low when going to an exhibition.

## THE LAYOUT

The scenic part fits in two boxes and provides a diorama of 2.40 x 0.50 m.

It is digitally operated. The track is from PECO, so are the five points. But the turntable is home made, so is the mobile bridge. Buildings are mainly built from foam board, cardboard or plastikard. Two sidings allow some movement and change of rolling stock. The layout has been designed to be linked to further show-cases if we decide to do so in the future.



L9 Taw Magna

## L9 Taw Magna

**Ilford and West Essex O Gauge Group**

**Dimensions: 12.5m x 3.05m**

This small terminus layout is based on a West Country location set in the fifties. This gives us an opportunity to exhibit a wide range of locomotives, mostly GWR, from the 1930s up to the end of BR steam. Both SR and LMS types may be seen occasionally. Diesel traction can also make an appearance.

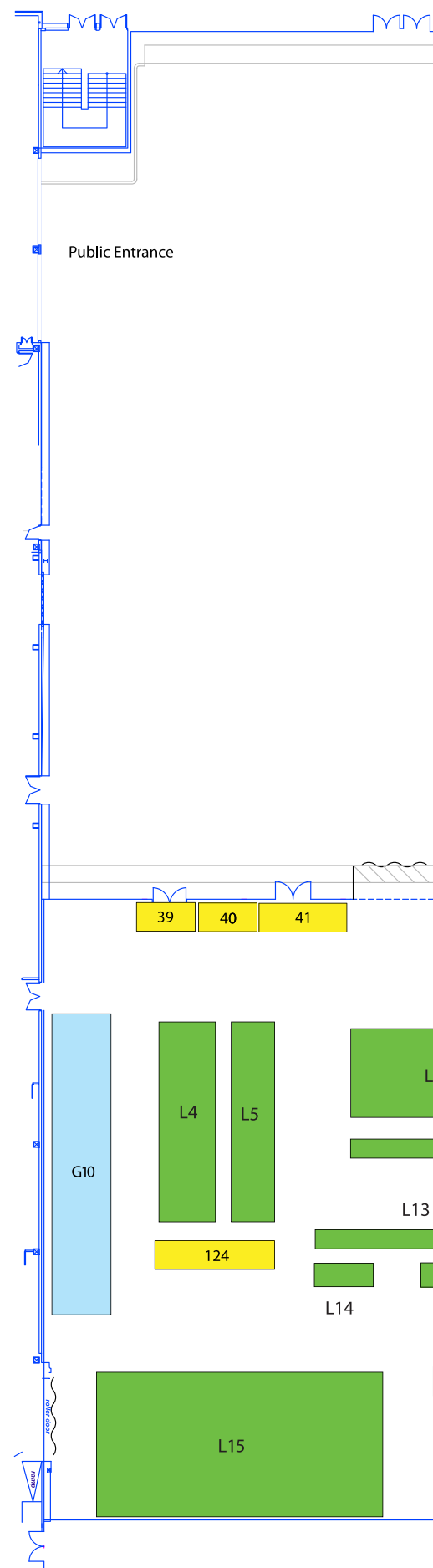
Most of the stock has been built by club members from well known kits with the odd couple of scratch built specimens. Malcolm Mitchell, Finney, Scorpio, Connoisseur and Javelin are some of the loco kits that have been used.

The prototypes for the main buildings came from around the Devon area (you may recognise Abbotsbury) and are all scratch built. A large turntable has been added to allow larger locomotives to visit - not strictly realistic but it gives us an opportunity to run an interesting variety of locomotives at exhibitions.

# Trade Stands

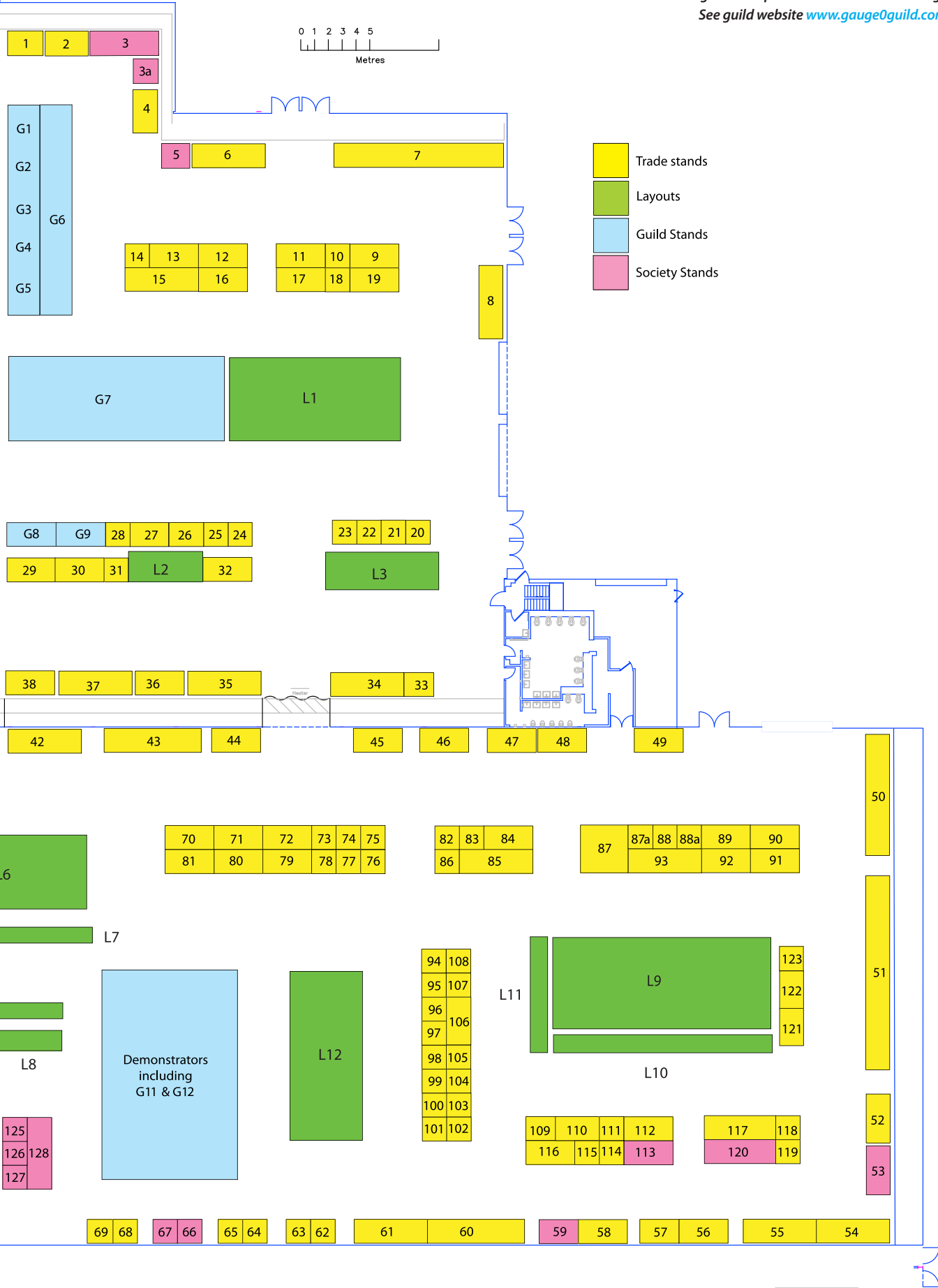
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# Guildex2012 floor



# plan

This guide and plan were correct at time of going to print.  
See guild website [www.gauge0guild.com](http://www.gauge0guild.com) for updates



## Demonstrators

<b>Ian Rathbone</b>	Painting and lining practice.
<b>Derek Mundy</b>	Signalling & soldering school
<b>Ruth Redgwick</b>	Figure painting
<b>Martyn Welch</b>	Weathering technique
<b>Fred Lewis</b>	Loco building from brass kits
<b>Sandy Harper</b>	Using a R S U and soldering skills
<b>Norman Solomon</b>	Assembly of C & L trackwork
<b>Alan Hunt</b>	Putting together Ian Kirk coaches
<b>Brian Daniels</b>	Chipping DCC diesel locos
<b>David Wright</b>	Buildings from scratch
<b>Nick Baines</b>	Workshop practice, marking out etc
<b>John Cockcroft</b>	Assembling kit built valve gear
<b>Charlie King</b>	Assembly of Slaters plastic wagon kits

**If you wish to register for a hands on session, and have not already done so via the newsletter, go to Derek Mundy on the Demonstration stands.**

*Also in the demonstration area will be the DCC demonstration and the Loco Clinic*

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- G2** Guild Information
- G3** Area Constituency Representatives
- G4** Overseas Representative
- G5** Video Library
- G6** Technical/Competition/Wagon Construction Workshop
- G7** Bring & Buy
- G8** Guild Sales
- G9** Publications
- G10** Executor & Trustees
- G11** Loco Clinic, Peter Silvester, John Edwards and Nigel Smith give advice on locomotive construction with particular attention to chassis and smooth running.
- G12** DCC demonstration by Barry Sumsion and Jeff Rodway. All aspects of DCC are covered.

## MODELLERS HELP DESK AND COMPETITION STAND

Run by the Technical Committee we give hands on advice about general modelling problems, including information about motors and gearboxes, locomotive construction and workshop principles, We can help with wiring problems including DCC chip fitting. We run a hands on soldering workshop available on request.

**NB Competition stand models are covered by Guild insurance, models brought in on the day are at the owners risk.**

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L9 *Taw Magna*



L10 *Milford*



L10 *Milford*



L10 *Milford*

All pointwork and signals are electrically operated. The pointwork is made from C&L components. The balance of trackwork is Peco. The fiddle yard uses cassettes to marshal stock.

The tunnel mouth scenic section was added in 2010. All the scenic work has been modelled with woodland scenic flock, garden dirt and static grass using a Noch Grassmaster applicator. Trees are also handmade from reinforcing dried garden flower stems and applying flock. The larger trees are made from wire armatures coated in Treemendus bark, again coated with various flock/leaf mixtures. Future work may include a small individual yard which can be added in place of the turntable and shed board to give a little variety of operation at shows.

We operate this layout for enjoyment so don't be surprised to see the odd train of unexpected stock arriving. During the exhibition we may run an 'anything goes' policy for a period to allow the club members to run their non GWR stock. Don't forget we are based in 'Great Eastern' territory and not all members are pro-GWR! This may not be prototypical, but does add variety at exhibitions.

We hope you enjoy the layout. Feel free to ask questions that you may have about the layout or the club.

## L10 Milford

**Rennie Lord, Normanton & Pontefract RMS**

**Dimensions: 12m x 0.8m**

Milford is a sleepy village located in the West Riding in Yorkshire. Agriculture has always played a major part in the industry of the area and, as the soils locally are suited to the growth of grain. Mills have been built and used for centuries in the area. Towards the end of the 17th century, Addison Cockerill built a new water mill on the local stream. This took water from higher up the stream via a small canal and fed an overshot wheel. Later, with the coming of the industrial revolution, this fell into disuse and disrepair as the canal was severed to bring the North Eastern Railway to Milford. The mill has now been taken over by his descendants and is used as a store for their thriving removal and timber business. Although there are numerous mines in the area, Milford has none of its own and coal has to be brought into the village. To this end, the railway built a coal drops in the goods yard which also boasts a brick built goods shed. Cattle and sheep are taken to market from Milford and a small cattle pen facilitates this.

Milford is based on North Eastern Railway practice in 1920-30s. Most of the original buildings are from the Garforth and Kippax area although the signal box is from Kirkham Priory.

Each of the six scenic baseboards are 4ft long and 32ins wide made of 9mm ply utilising a fixed backscene as 'backbone' of the layout, with the integral hinged legs. The track base is 6mm ply with a layer of cork glued onto it to provide a shoulder for ballast and some believe it makes for quieter running. There are 6ft turntables at either end of the layout, making the total length 40ft.

The trackwork in C&L Finescale parts. The points were hand-built but most of the lengths of track are C&L flexi-track with keys individually added. The point and signal motors are all operated using the DCC hand held controllers. Routes can be set by adding a short code and then this causes several points and signals to move simultaneously. Interlocks are not as reliable as the full scale versions of automatic signalling and accidents have been known!

The signals are mostly scratch-built using wood but the arms are from Model Signal Engineering.

The scenery on the layout is formed using polystyrene blocks, shaped then glued on, then covered in filler Artex is cheaper than proprietary fillers. This was then painted grey/brown colour before being coated with white PVA glue. A mixture of 'Flocking' fibres were then 'puffed' from a 'NOCH' puffer bottle and to set. After several layers of flock had been applied ground foam was used to cover the flock to simulate weed growth. Finally a beige coloured teddy bear fur was used to simulate tufts of longer grass. There are few dried flower bits and pieces here and there. The river was then painted with acrylics and many layers of varnish added.

The buildings are made from 6mm plywood shells covered with Slater's plastic brickwork suitably painted and weathered using enamel paints and weathering powders.

The telegraph wire is very thin latex ('Z-line'). It is very forgiving but must be strung up afresh for every show. The poles are permanently fixed into the baseboards. All locos are fitted with DCC chips to enable simple independent control with no need for isolation sections except on the turntables.

I hope you enjoy watching the layout. If you have any questions please don't hesitate to ask the operators.

## L11 Hemyock

**Eric Hines**

**Dimensions: 8.6m x 3.7m**

The Culm Valley Light Railway Company was a local enterprise which was formed in 1873 to build a standard gauge light railway from Tiverton Junction, on the then broad gauge Bristol and Exeter main line, 7 1/2 miles to Hemyock with intermediate stations at Uffculme and Culmstock.

The railway received Royal Assent on the 15th May, 1873 and construction began the following year, but progress was slow and the contract with the first builder was cancelled. Another builder was appointed but he met with problems, not the least of which was losing many of his workmen to the local farmers at harvest time; however, the line was eventually completed and finally opened on the 29th May, 1875 by which time the mixed gauge had reached Exeter and the regular working of standard gauge goods trains made through freight traffic possible.

The line was worked by the Great Western, this company having taken over the Bristol and Exeter Railway, who were to have worked the line, just prior to the opening of the CRV.

The line was lightly constructed and followed the banks of the river and existing land boundaries for most of its length, but the severe curves resulting imposed an overall speed restriction of 15mph and seriously restricted the choice of motive power and rolling stock.

The railway soon ran into financial difficulties, passenger receipts dropped and the Great Western somewhat unwillingly purchased the line in April 1880, later opening halts at Coldharbour, 23rd February, 1929 and Whitehall, 27th February, 1933.



L11 Hemyock



L11 Hemyock



L12 Avon Bridge Test Track



L12 Avon Bridge Test Track

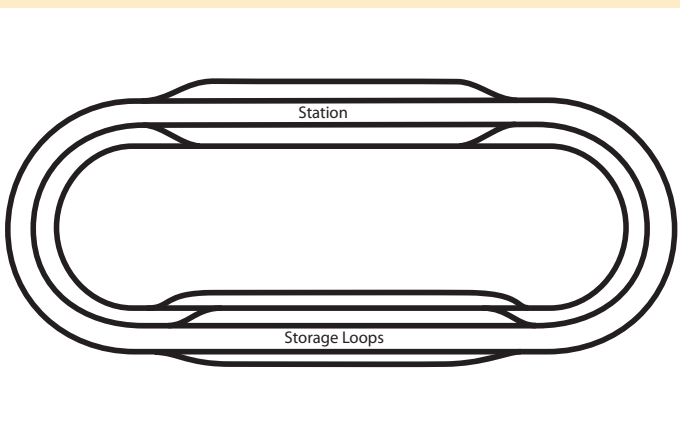
## L12 Avon Bridge Test Track

**Solihull MRC**

**Dimensions: 3.9m x 7.9m**

Originally conceived as just a circle to cater for the 0 gaugers in the club to see something running. It was later extended by inserting eight boards between the split circle to add interest in the operation of the layout. We now have what in effect is a cross between a tail chaser and a test track but with proper railway operation in a limited form. It is interesting to operate and achieves our goal of watching trains go by.

The station building is based on the Midland Railway at Northfield and is scratch built. The stock is owned by the club members and we try to keep to 1930s and 1960s for operation purposes. Scenery is not yet complete but we hope it will give an indication of where we are going.



## L13 Marske

**Martin B Smith**

**Dimensions: 9.7m 1.8m**

Marske is situated on the Darlington to Saltburn line one stop before the line terminates at Saltburn and was founded by Henry Pease in 1859 as part of an extension of the North Eastern Railway. The station opened in 1861 and is still in use today although much has changed.

The layout is based on the original track plan but has been modified to a small extent due to space constraints. Photographs unearthed during research provided the information required to model the various buildings including the early North Eastern footbridge.

Research took about three years and building commenced in 2005. The layout was built



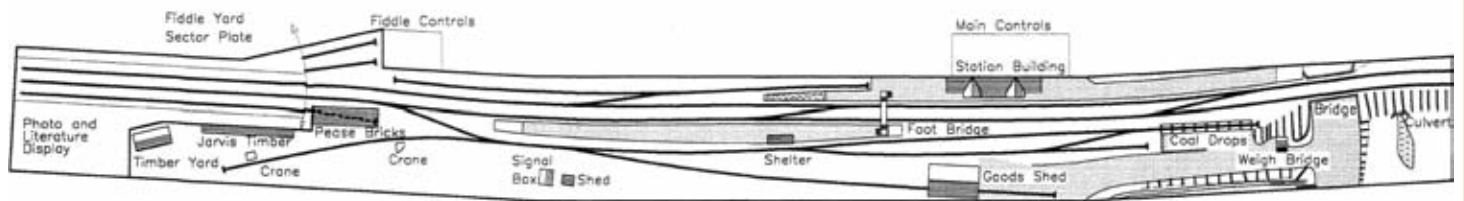
L13 Masrke

in my kitchen, board by board. I do not have the space to assemble the whole layout at home and was therefore fortunate to be invited to a show to demonstrate the model as a part built in 2006. The scenics were then completed at home. I now have the advantage having joined Cleveland Model Railway Club to assemble the layout for testing in the club premises, a distinct advantage when taking the layout to a show. There is a good chance that layout operation will be trouble free but not a guarantee. Bouncing a layout down a motorway to the venue may still produce gremlins.

The layout breaks down into seven sections, the longest being the three road sector plate. The boards are 9mm ply sandwich framework with 6mm MDF tops cut away and contoured in areas such as the coal drops and road.

Trackwork is Peco Streamline Code 125 Bullhead and pointwork is mostly built from Marcway kits. Points motors are H & M SM3s sourced from various club sale stands. Electrics are conventional and operation of the main layout is by hand held controllers. The fiddle yard is operated by a separate H&M controller.

The signals are operated by servos and were built by Peter Simmerson who introduced me to the O Gauge Guild. He usually operates the layout with me.



L13 Masrke

The buildings are built mainly from plain and embossed styrene. I made separate wooden transportation boxes for each building and feel it was worth the effort to protect them from damage considering the work that was put into their construction. The only structures on the layout which are not authentic are the brick company and wood yard which mask the sector plate. Authentic local names have been used but this area is fictional.

## Rolling Stock

The majority of the locomotives running on the layout have been built by myself from etched kits. These are mainly North Eastern/LNER modelled on the full sized versions on shed at Saltburn, Middlesbrough, and Darlington during the 1920s/30s. As I improve my skills I am hoping to bring the layout back into North Eastern days. Painting and lining requires more practice also more scratch building will be involved. Coaches are built from Ian Kirk parts kits for LNER teaks and Connoisseur brass etches for North Eastern four wheelers. The Cowan and Sheldon Crane brass etch kit is from D&S and the rest of the rolling stock kits are from well known suppliers.

We enjoy operating the layout at the shows and hope that the enthusiasts and friends who attend the shows also enjoy its operate. I am still interested in any information that may come to light concerning Marske especially details on a building not included on the layout called the paint store or lamp store.



L14 Rhydwnn Fawr

## L14 Rhydwnn Fawr

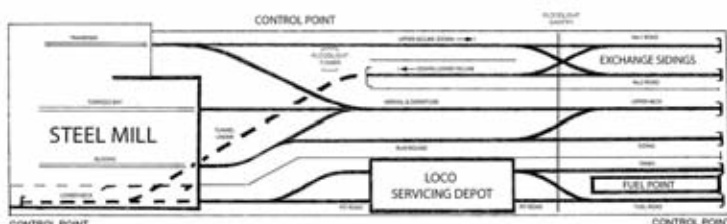
Ian Blackburn

Dimensions: 4.2m x 0.8m

Rhydwnn Fawr (pronounced: 'hri-dwin-vower') or 'Great Whiteford' when translated into English, is set in a corner of a fictional South Wales steelworks.

The inspiration for this 4.2m x 0.8m (13ft9in x 2ft8in) layout came from two sources – the first was a layout (the title of which I can't remember) that appeared at a Guild Event at Bletchley in the late eighties. It was a stud contact, multi-level affair, about twelve feet long with three or four levels, each of which was connected to the next by an incline, which ended in a headshunt that was only long enough to accommodate a small locomotive plus a couple of 10ft wheelbase wagons. Very simple, but it kept me entertained for hours on end, and I distinctly remember the squeals from the current collection skates on the locos as they went over the pointwork – the second inspiration came from a compact '00' scale layout called 'Iron Mould Lane,' which features a steelworks. Although only on one level, it proved an immense source of fascination for me, and the level of details that was on it, had to be seen to be believed! Rhydwnn Fawr is therefore a combination of these two projects.

Set on two main levels, with a pair of exchange sidings set on an intermediate level on the right-hand side at the rear of the layout, the top level is dominated by the large steel mill on the left. On the lower level at the front, is a small loco servicing depot, plus a



# Layouts

fuelling point to its right – both of these two items are fitted with genuine miniature fluorescent lights!

A fleet of freelance industrial diesel locomotives haul all of the trains on this layout. They appear from the 'rest of the works' behind the mill on the left, emerging just in front of the mill's two huge air-purging fans. Trains that consist of one of the site's huge 'torpedo' wagons full of molten iron from the blast furnaces are drawn out onto the upper level until they are standing above and behind the servicing depot on the low level. Here, you will be able to see the flow in the 'runner' and 'riser' vents in the top of the wagon from the blisteringly hot metal that is being transported within the wagon's barrel. After the huge sliding door of the mill is raised, the wagon is propelled into the building ready for discharge. After discharge, this procedure is reversed, with the loco either running round the wagon or a fresh loco being attached at the other end. The bay next to the one used by 'torpedo' wagons is used for delivering and dispatching wagons after they have been loaded with finished steel blooms. All this plus the many movements of locos and wagons of fuel and parts to and from the servicing depot below makes Rhydwyn Fawr a very busy location.

This layout has been designed so that its operators can interact as closely as possible – so, if you have any questions about this layout, please do not hesitate to ask one of us!

## L15 Dainton Bank

**Michael Heaven**

**15m x 7m**

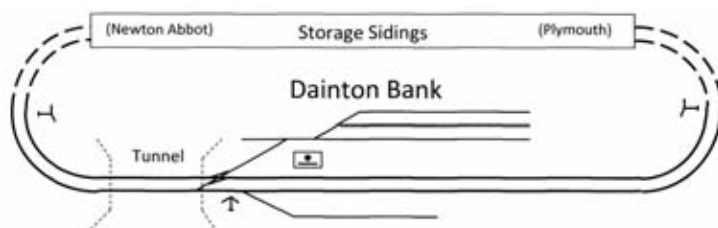
This excellent exhibition layout has been on the circuit for a number of years, providing the opportunity to see scale length trains both passenger and freight, hauled by correct locomotives, all superbly detailed and mostly professionally painted, as they represent the climb up Dainton Bank in Devon and disappear into Dainton tunnel.

The layout is 0 gauge and has featured in various magazines over the years. Some trains are banked, particularly freight. The banker drops off at the summit to enter the sidings before gently returning down the bank. Traditionally this layout has been operated by conventional 12volt DC. This year it has been converted to DCC and consequently operation of the banker will be even slicker as well as providing the realistic sounds of double headed expresses struggling to lift their heavy load over an important part of the Great Western Mainline.

The member of the group that built and operate this layout have a great sense of humour. Look out for the cameos of typical daily life, as it existed over 50 years ago. The gently rocking caravan, with 'just married' scrawled on it, never fails to bring a knowing smile when realisation suddenly dawns on the face of the onlooker. The young ladies bathing in the pond provide an excellent opportunity for a young boy to have a peep only to be chased by the local village bobby. Similarly some of the signs are taking the 'mickie' out of the operators.

Being 48ft long with visibility on only three sides means plenty of viewing space to see trains in their natural environment. There is the added excitement of not knowing what is being kept in the hidden storage sidings until suddenly a train comes into full view. It may be a humble pannier tank and a couple of wagons, or alternatively it may be a double headed King and Castle with holidaymakers excitedly looking out of the windows as they travel to the West Country for their annual seaside dip.

No sooner has one train gone, than off comes the signals and slowly a 2-8-0 with a load of mineral wagons behind it chugs slowly into view and at the rear is a tank engine helping to give it a push up the famous incline. Come and see the layout in reality and we are sure you will not be disappointed when you see finescale 0 gauge modelling at its best.



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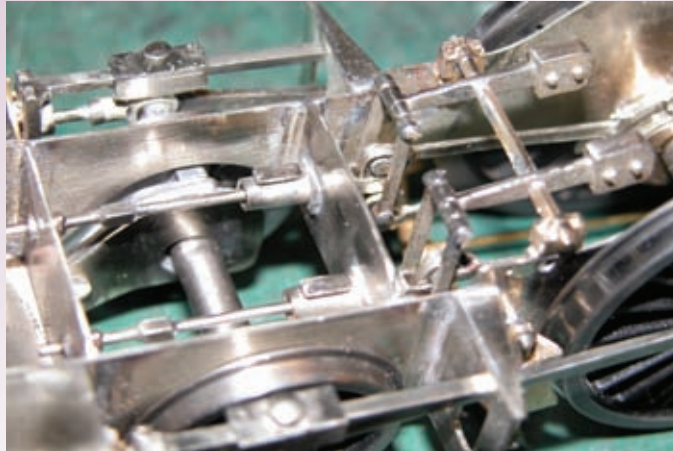
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# K.F.T. Design

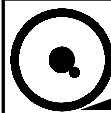
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**Later:** 32' 4-Compt 1st diagram 2Q2 • 32' Luggage Composite diagram 2K2A

**August:** GCR/LNER 45' 15T Bogie Fish Van diagram 50/50A

This advertisement was prepared in early June, so please check current status

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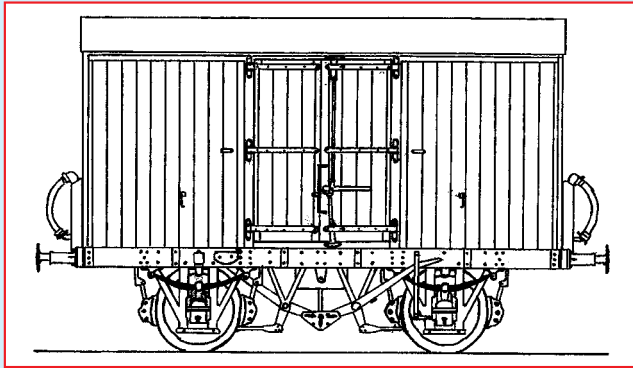
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